

Glendale Downtown Development
Design Standards Manual

City of Glendale



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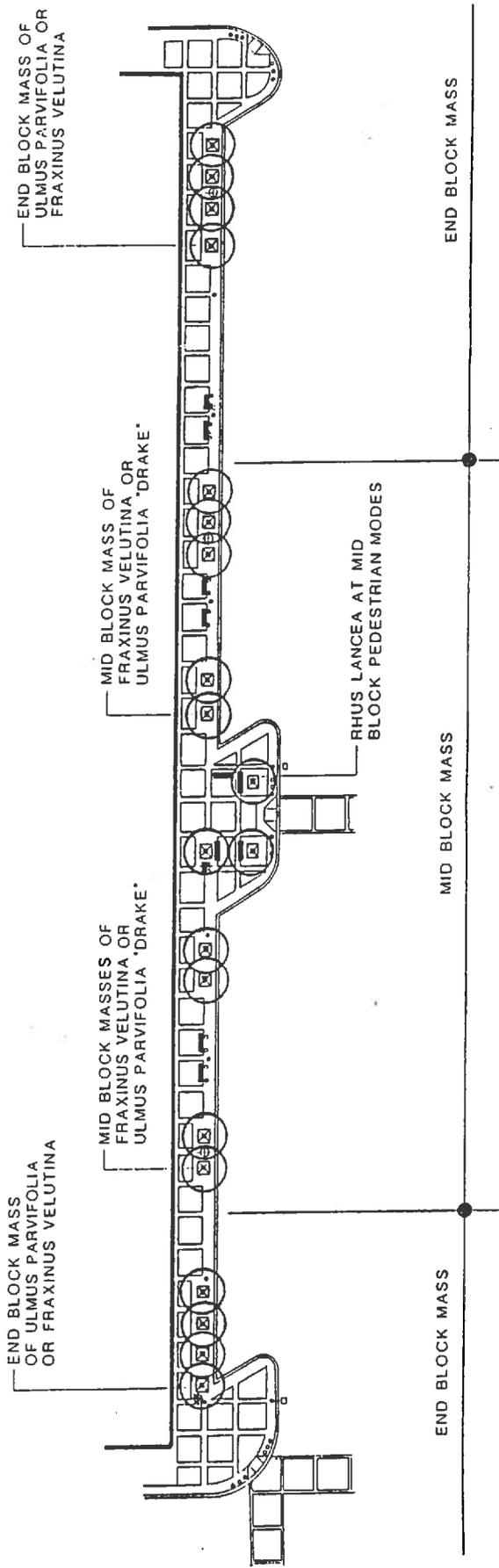
Downtown Streetscape Standards

The Glendale Downtown Streetscape Standards are to aid developers, designers and the City of Glendale in the planning and design of the downtown right-of-way. These standards are not intended to replace existing design guidelines, ordinances or standards. They are to be used as a supplement for consistent design of the right of way within the specially designated area of downtown Glendale. The design standards outline specific design treatments but are not intended to supersede sound engineering judgement.

These design standards should be used in conjunction with other city specifications, standards, guidelines and ordinances during both new development and revitalization of downtown properties.

The Streetscape System

The downtown streetscape system is based on a modular brick pavement pattern made up of eight foot square brick panels and salt finish concrete headers. A three foot wide concrete band along the curb allows for the installation of the street furnishings with sufficient clearance from diagonally parked cars. Street furnishings are placed in unison with the brick pavement pattern and they are zoned predominantly along the three foot wide concrete band bordering the curb. The crosswalks align with the pedestrian ramps and brick panels where possible. Concrete headers encircle the tree grates and align the street furniture elements such as lights and signs. Benches are placed as linear seating along sidewalks combined with bollards, and in groups facing each other or at ninety degrees to each other for pedestrian seating nodes. Bollards are placed between benches and the curbs for added safety and at intersections to direct pedestrian movement on to crosswalks. Street trees are used for mass at block ends, at mid blocks, pedestrian nodes and at transit stops. They are placed in tree pits with manufactured tree guards and custom designed tree grates. News stands are to be consolidated per block. Phone booths are placed between and aligned with street trees. Bus transit stops, including the mid block and far side intersections are designed for articulated buses and based on the eight foot brick panel system. Dated pedestrian light fixtures are aligned by the concrete header and alternate with the tree masses and street lights when possible.



- BRICK PANEL
- ⊠ TREE, GRATE & GUARD
- DATED PEDESTRIAN LIGHT
- ⊞ BENCH/BOLLARDS
- *** BOLLARDS AT X-WALK
- PHONE BOOTH-TRASH RECEPTACLE
- ⊙ STREET LIGHT
- * ASH RECEPTACLE

FIG 1 BLOCK PAVING/FURNISHINGS LAYOUT

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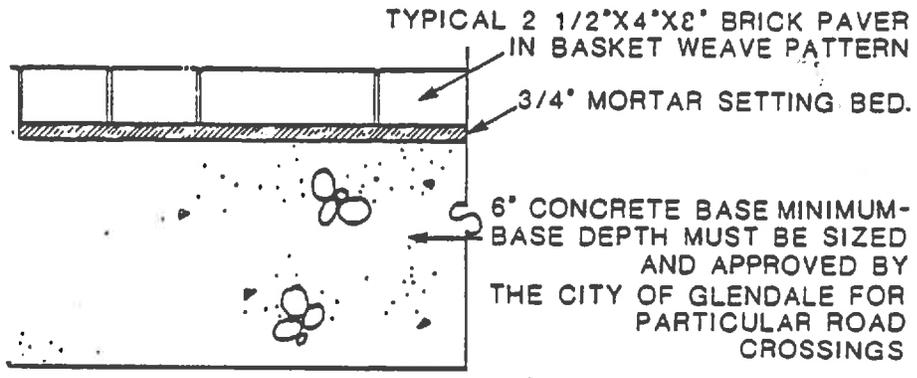
Pedestrian Brick Sidewalk

Glendale Downtown typical sidewalk is composed of eight foot square brick panels, one foot wide salt finish concrete header between the panels and adjacent to properties with a three foot wide salt finish concrete band bordering the curbs. Concrete bands encircle tree grates as shown in the detail and are used to provide a concrete surface for the installation of street furniture elements such as lights, receptacles, and bollards. The existing physical condition of soils must be verified prior to using the typical brick cross section. The Typical brick cross section is the minimum concrete depth allowed with normal soil conditions such as encountered at 58th Avenue and Glendale.

Models: Brick to be as manufactured by El Paso Brick Company. Model 2 1/4" x 4" x 8" modular brick pavers Indian River Red passing ASTM Test, C87 as slightly efflorescent. Mortar setting bed to be Portland Cement mortar with laticrete 3701, as per manufacturers recommendations. Sand for filling joints shall be the silica sand (16 grits).

Manufacturer:

El Paso Brick Company
Post Office Box 12336
El Paso, Texas 79913-0336
(505) 589-4474



TYPICAL BRICK CROSSWALK CROSS SECTION

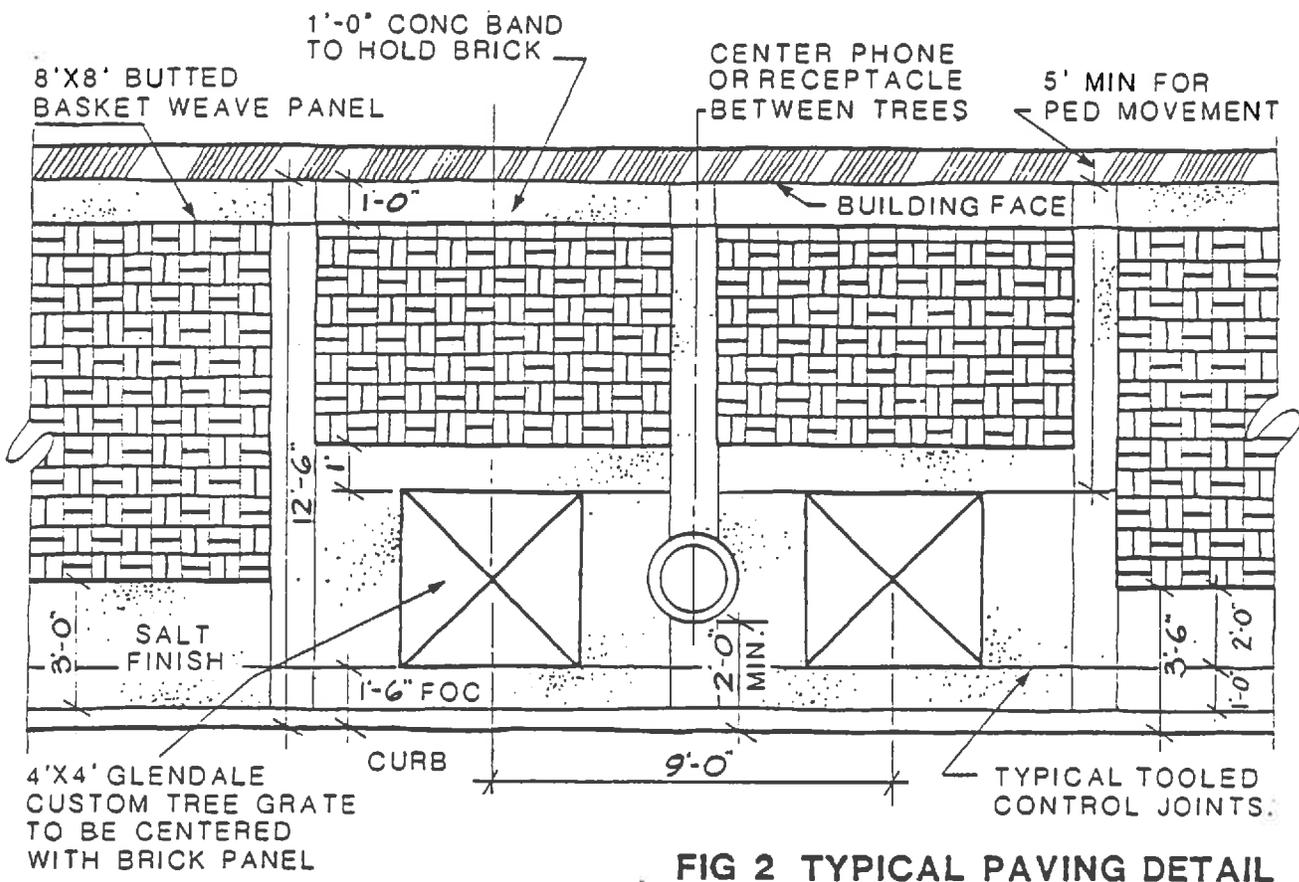


FIG 2 TYPICAL PAVING DETAIL

GLENDALE DOWNTOWN DEVELOPMENT DESIGN STANDARDS MANUAL

City of Glendale



Benches

Teak wood benches provide pedestrian seating nodes and transit waiting areas. Benches are placed in three basic ways:

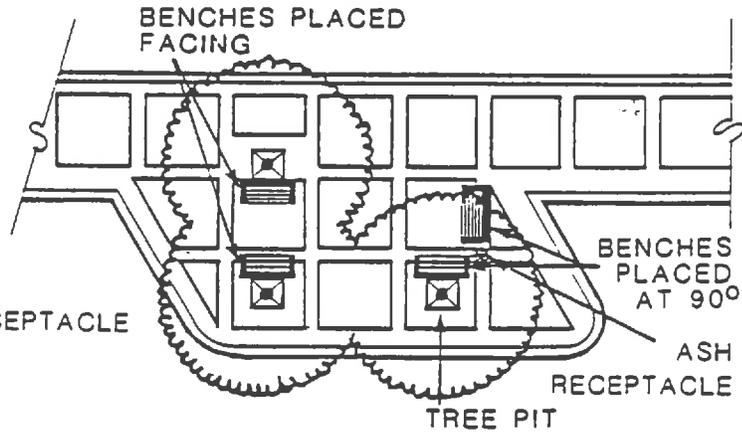
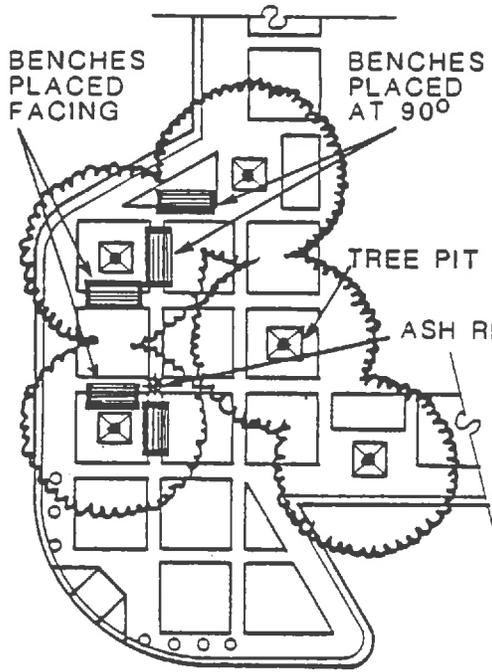
- 1) As linear seating along sidewalks with bollards, facing away from streets.
- 2) In groups facing or at 90 degrees to each other for pedestrian seating areas.
- 3) As transit stop furnishings, grouped facing or at 90 degrees to each other whenever possible.

Models: Benches are to be as manufactured by British American Marketing six foot long mid-dip teak bench. Model number MD6.

Color: Natural wood color.

Manufacturer:

British American Marketing Services
251 Welsh Pool Road
Unionville, Pennsylvania 19475
(215) 948-4000



BENCH PLACEMENT TO PROVIDE SEATING AREAS

BENCH PLACEMENT TO PROVIDE SEATING AREAS

MENDIP
TEAK BENCH

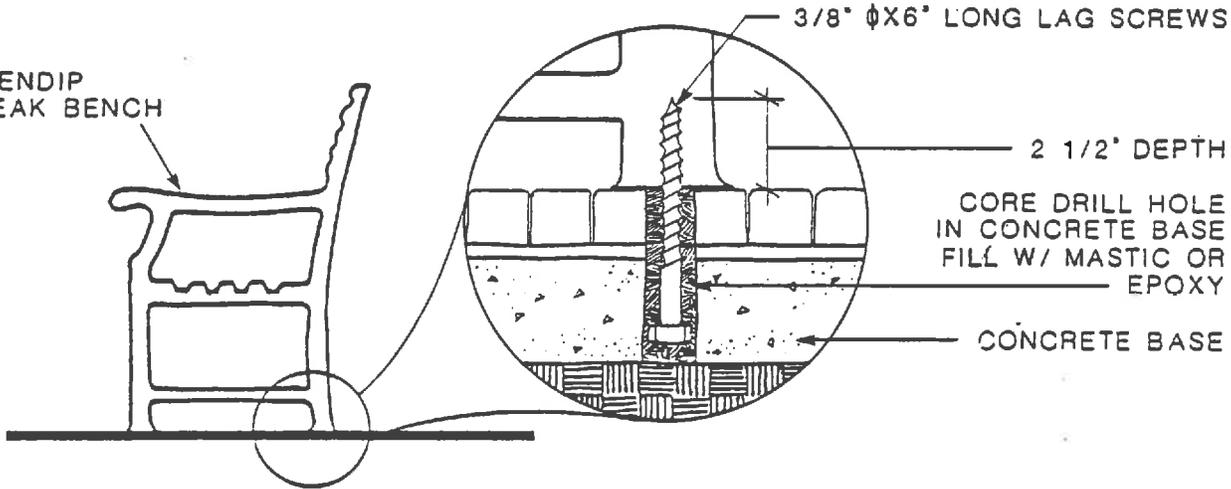


FIG 3 TYPICAL BENCH DETAIL

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City of Glendale



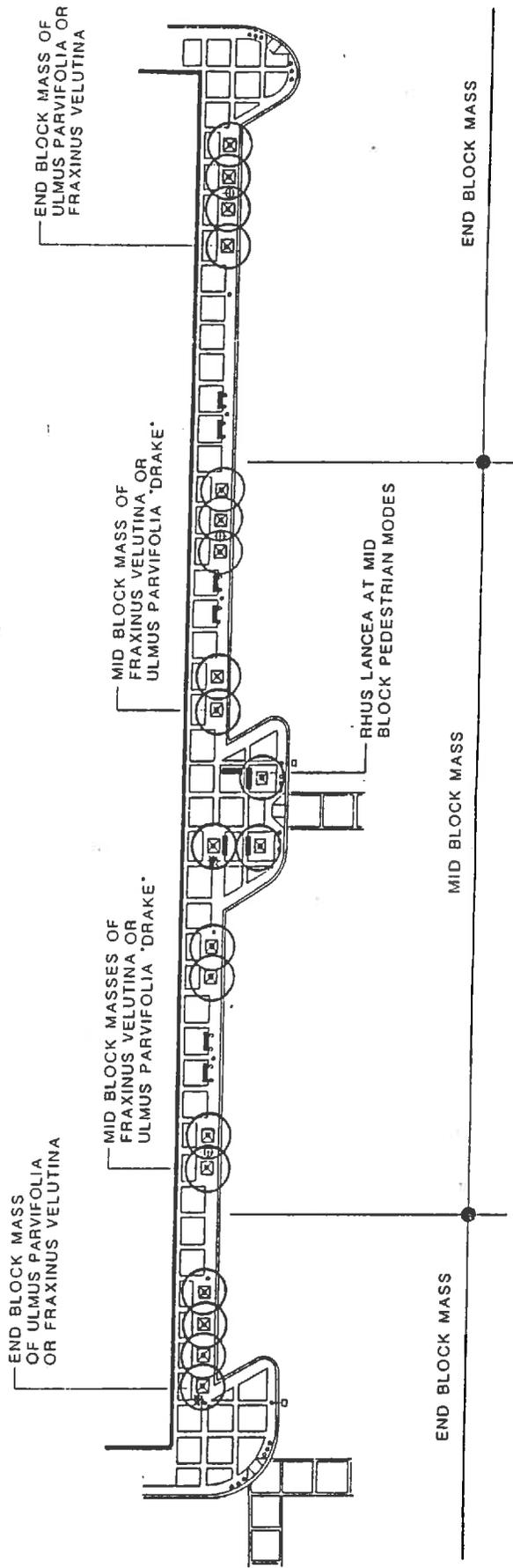
Street Tree Placement

Street trees are placed in masses of two or more as illustrated in the diagram. Trees installed in the Glendale downtown sidewalks must be installed per the requirements of the street tree standard detail and be a minimum of a thirty six inch box size per current American Association of Nurserymen standards.

Tree Placement: Blocks are to be treated as follows: End block masses of three or more trees spaced in consecutive pavement pattern sequence of nine feet on center. Three mid block masses of two or more trees spaced at nine to eighteen feet on center. Mid block pedestrian nodes[when present] massed at eighteen feet on center. Consideration should be given to the adjacent building facades, canopy, and signage requirements.

Required Tree Varieties:

End Block Mass: *Ulmus parvifolia* 'Drake' *{Preferred}* or *Fraxinus velutina* 'Rio Grande'. Mid Block Mass: *Fraxinus velutina* 'Rio Grande'*{Preferred}* or *Ulmus parvifolia* 'Drake'. Mid Block Pedestrian Nodes: *Rhus lancaea*



- BRICK PANEL
- ⊗ TREE, GRATE & GUARD
- DATED PEDESTRIAN LIGHT
- ⊞ BENCH/BOLLARDS
- ⋯ BOLLARDS AT X-WALK
- ⊕ PHONE BOOTH-TRASH RECEPTACLE
- STREET LIGHT
- * ASH RECEPTACLE

FIG 4 STREET TREE USE AND PLACEMENT

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Street Tree Pit

Tree pit composed of concrete structural system ,tree grate, tree guard, soil backfill mix and root chimney. Soil backfill mix composed of one to one ratio of soil and ground fir bark. The root chimney is composed of a 18" deep vertical excavation at the bottom of the tree pit filled with 18" of pea gravel. Concrete structural system to meet Maricopa Association of Governments uniform standard specifications for Public Works construction. Specifications for 2,500 psi concrete.

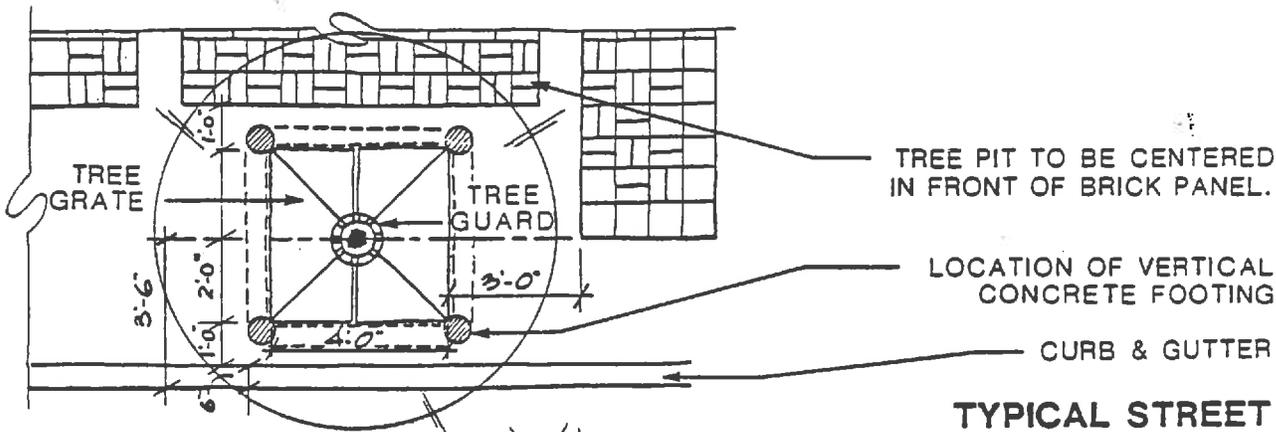
Models: Tree guard and tree grate as manufactured by Neenah Foundry Company. The treeguard is model number style A, 6' height for 14" ID tree opening with factory applied rustic red primer. The tree grate is model number - City of Glendale custom casting four foot square.

Color: Tree guard color to be a flat black U-1083 per Sterling Chemical Coatings Color Guide 1350 Brannon Avenue, St. Louis, MO 63139 (314) 776-4450. Tree grate color to be natural cast iron.

Paint: All work shall conform to the recommendations of the Painting and Decorating Contractors of America for Type 1 work. Ungalvanized metal shall have: 1 coat Palgard Epoxy Coating Metal primer and 2 coats Palgard Epoxy Coating. Pratt & Lambert has been used herein as a standard of quality. Equivalent products of other manufacturers may be used upon approval.

Manufacturer:

Neenah Foundry Company
Box 729 2121 Brooks Avenue
Neenah, Wisconsin 54965 (415) 725-7000

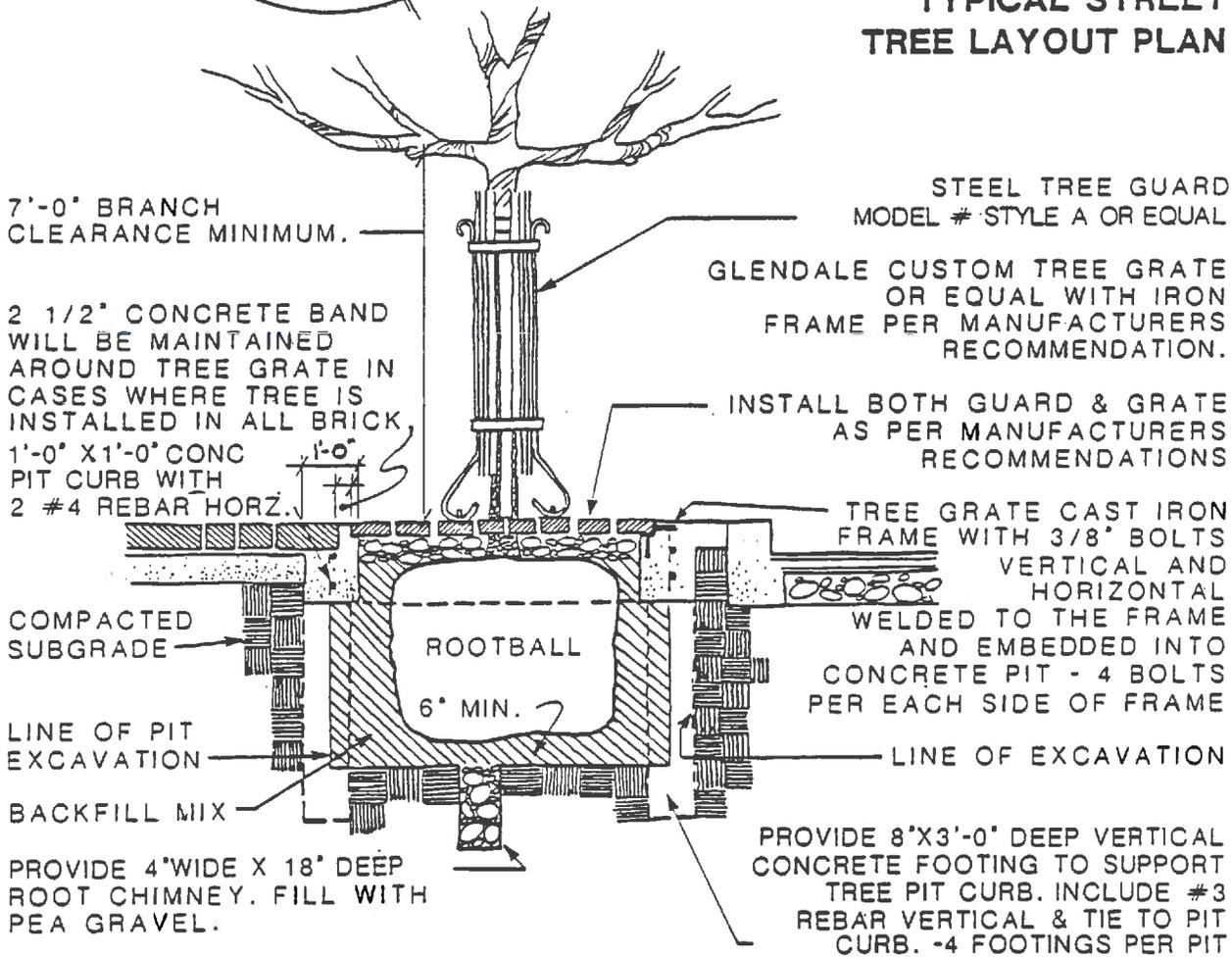


TREE PIT TO BE CENTERED IN FRONT OF BRICK PANEL.

LOCATION OF VERTICAL CONCRETE FOOTING

CURB & GUTTER

TYPICAL STREET TREE LAYOUT PLAN



7'-0" BRANCH CLEARANCE MINIMUM.

2 1/2" CONCRETE BAND WILL BE MAINTAINED AROUND TREE GRATE IN CASES WHERE TREE IS INSTALLED IN ALL BRICK. 1'-0" X 1'-0" CONC PIT CURB WITH 2 #4 REBAR HORZ.

STEEL TREE GUARD MODEL # STYLE A OR EQUAL

GLENDALE CUSTOM TREE GRATE OR EQUAL WITH IRON FRAME PER MANUFACTURERS RECOMMENDATION.

INSTALL BOTH GUARD & GRATE AS PER MANUFACTURERS RECOMMENDATIONS

TREE GRATE CAST IRON FRAME WITH 3/8" BOLTS VERTICAL AND HORIZONTAL WELDED TO THE FRAME AND EMBEDDED INTO CONCRETE PIT - 4 BOLTS PER EACH SIDE OF FRAME

COMPACTED SUBGRADE

LINE OF PIT EXCAVATION

BACKFILL MIX

PROVIDE 4" WIDE X 18" DEEP ROOT CHIMNEY. FILL WITH PEA GRAVEL.

LINE OF EXCAVATION

PROVIDE 8" X 3'-0" DEEP VERTICAL CONCRETE FOOTING TO SUPPORT TREE PIT CURB. INCLUDE #3 REBAR VERTICAL & TIE TO PIT CURB. -4 FOOTINGS PER PIT

FIG 5 TYPICAL STREET TREE DETAIL

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Bollards

Cast iron bollards are installed to provide pedestrian security and to direct pedestrian movement. Bollards are placed between benches and curbs for added safety and security. Bollards are placed at intersections to direct pedestrian movement on to crosswalk areas. Edge of bollard to be 2'-6" minimum distance from the face of curb. Placement of bollard should correspond to parking spaces when applicable.

Models: Bollards are to be as manufactured by Spring City Electrical. Model number is the Madison Bollard with tapped holes for chain eyebolt.

Color: Color to be flat black per Sterling Chemical Coatings Color Guide. 1350 Brannon Avenue, St. Louis, MO 63139. (314) 775-4450.

Paint: All work shall conform to the recommendations of the Painting and Decorating Contractors of America for Type 1 work. Ungalvanized metal shall have: 1 coat of Palgard Epoxy Coating Metal primer and 2 coats of Palgard Epoxy Coating. Pratt & Lambert has been used herein as a standard of quality. Equivalent products of other manufacturers may be used upon approval.

Manufacturer:

Spring City Electrical
Post Office Drawer A
Spring City, Pennsylvania 19475
(215) 948-4000

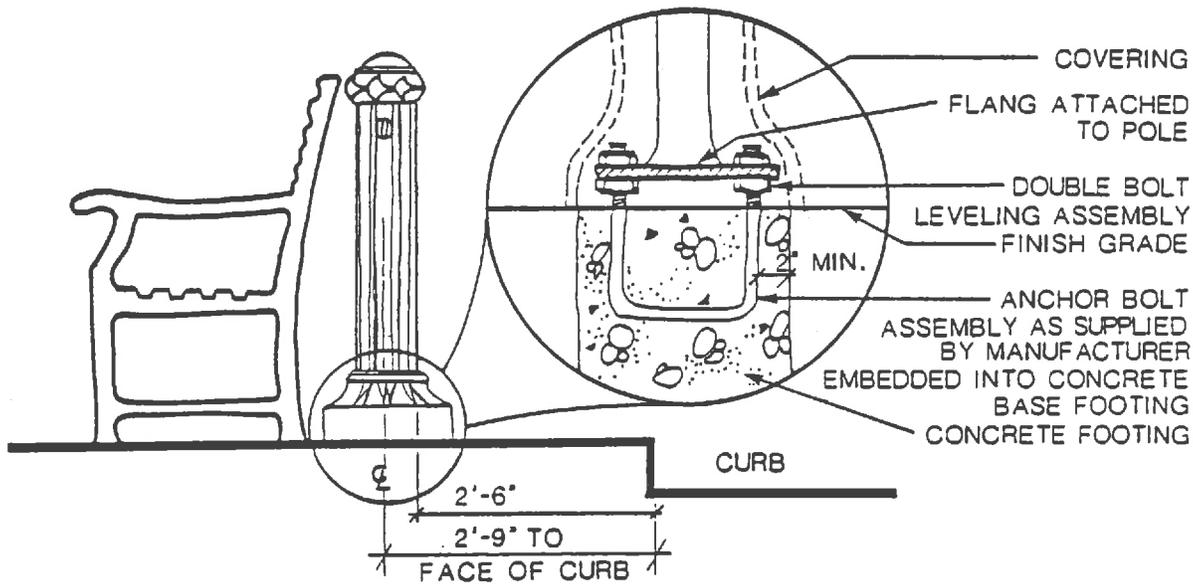
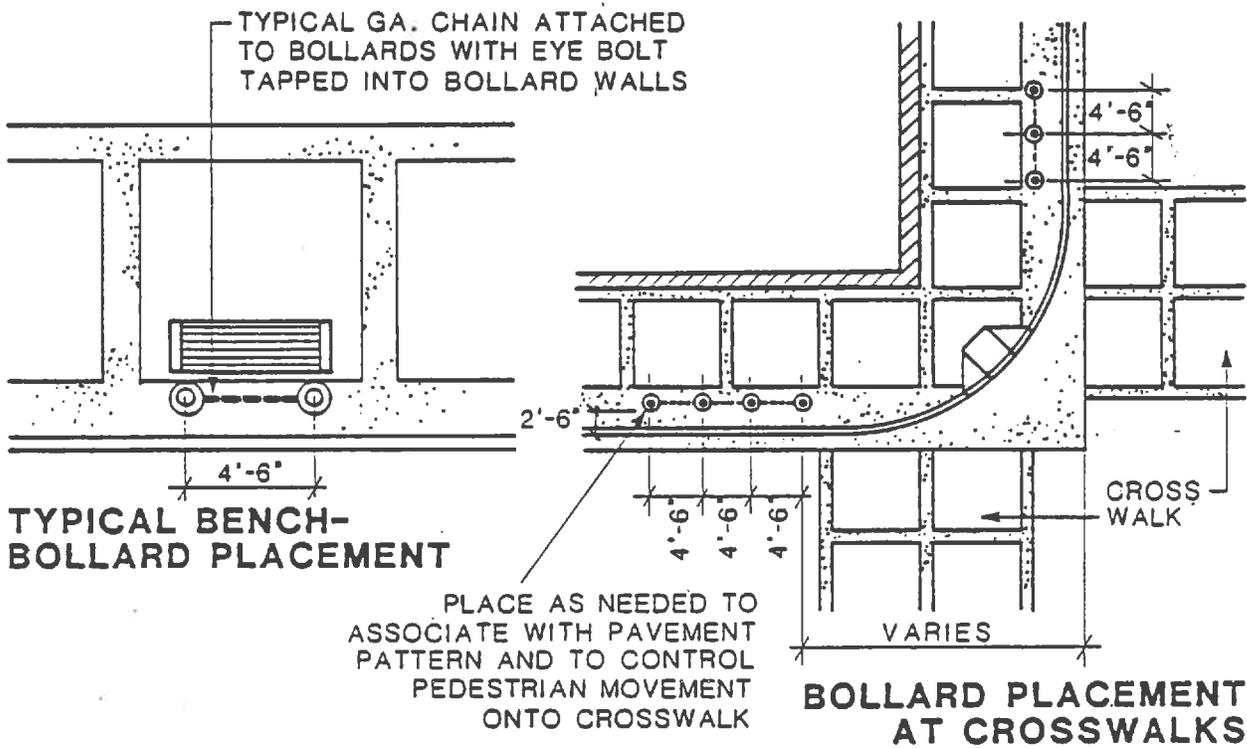


FIG 6 TYPICAL BOLLARD DETAIL

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Brick Pedestrian Crosswalks

Brick pedestrian crosswalks are provided at all intersections and at mid block responding to pedestrian trends and as deemed necessary by the City of Glendale. Crosswalks align with pedestrian ramps and sidewalk brick panels whenever possible. Eight foot brick panels with a one foot wide salt finish concrete header is the minimum pedestrian crosswalk width. Double panel crosswalks are recommended at all high pedestrian volume crosswalks as illustrated in details. The typical brick crosswalk cross section requires that concrete depth be modified as per road use and soil conditions. Final concrete base depth to be approved by the City of Glendale.

Models: Brick to be as manufactured by El Paso Brick Company. Model 2 1/4" x 4" x 8" modular brick pavers Indian River Red passing ASTM Test,C87 as slightly efflorescent. Mortar Setting bed to be Portland Cement mortar with laticrete 3701, as per manufacturers recommendations. Sand for filling joints shall be the silica sand (16 grits).

Manufacturer:

El Paso Brick Company
Post Office Box 12336
El Paso, Texas 79913-0336
(505) 5899-4474

PROTECTION ANGLES 3/8"X3"X3"
SET 1/8" BELOW TOP OF CONCRETE

TYPICAL 2 1/2"X4"X8" BRICK PAVER
IN BASKET WEAVE PATTERN

3/4" MORTAR SETTING BED.

1/2"X8" SQ HEAD
ANCHOR BOLTS ALTERNATE
AT 12" ON CENTER

6" CONCRETE BASE MINIMUM-
BASE DEPTH MUST BE SIZED
AND APPROVED BY
THE CITY OF GLENDALE FOR
PARTICULAR ROAD
CROSSINGS

TYPICAL BRICK CROSSWALK CROSS SECTION

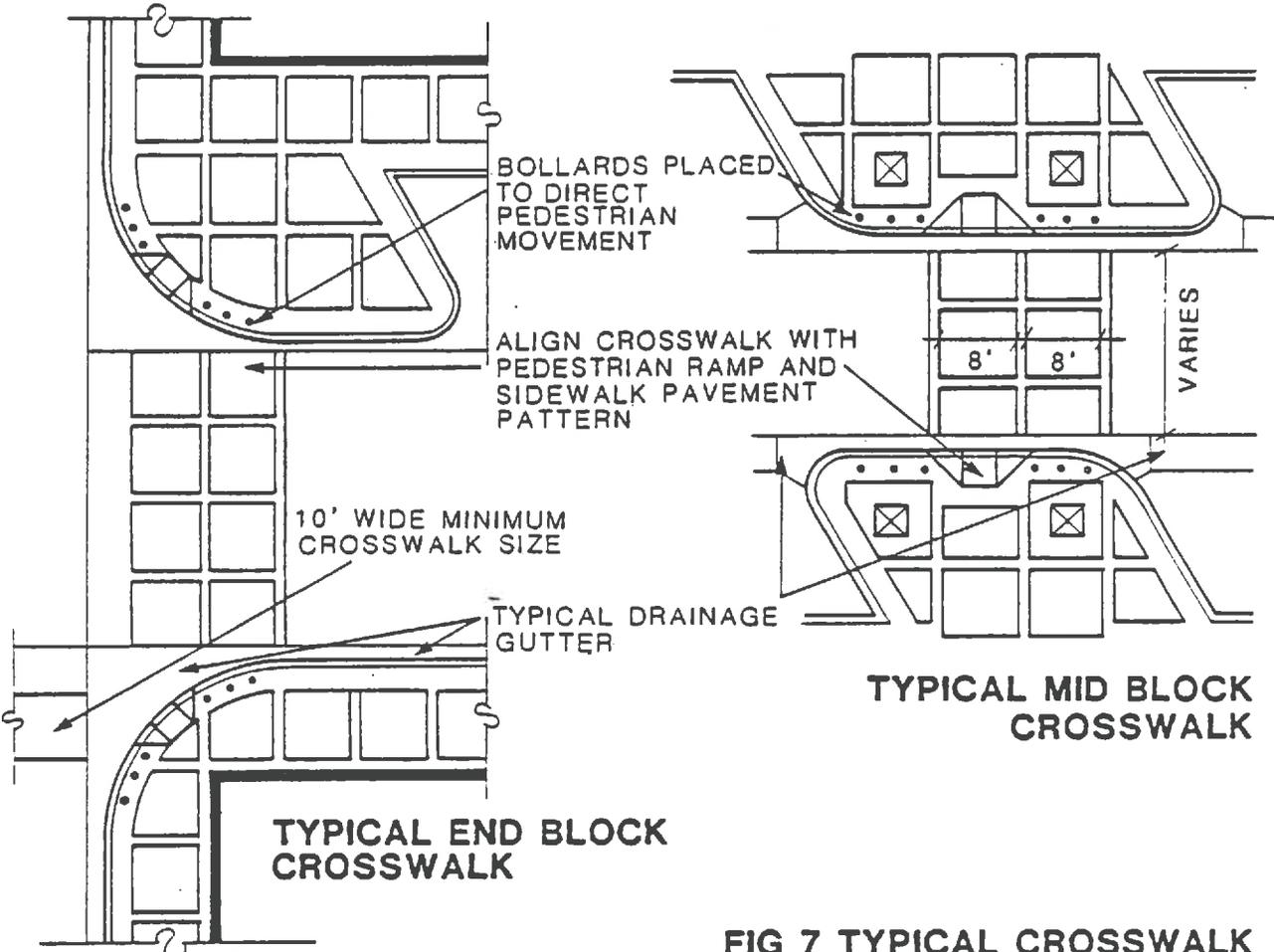


FIG 7 TYPICAL CROSSWALK

GLENDALE DOWNTOWN DEVELOPMENT DESIGN STANDARDS MANUAL

City of Glendale



Pedestrian Lights

Pedestrian lights are to be located not less than 25' from the adjacent intersection flowline, or as deemed necessary by the City of Glendale. Center pedestrian lights between trees, or mount light at similar tree spacing interval, in combination with street lights. Lights to be spaced at twenty seven to thirty six feet on center. Pole height shall be 10'-0". Edge of pole shall be a minimum distance of 2'6" from the face of curb. Pole location should correspond to parking spaces when applicable.

Models: As manufactured by Delta Lighting Systems, Inc. Model number HEOA 175 HPS Black with aluminum or steel pole with cast aluminum base model number PCB 104-10 or PCB 114-10 or equal as approved by the City of Glendale.

Color: Color to be flat black U-1083 per Sterling Chemical Coatings Color Guide. 1350 Brannon Avenue, St. Louis, MO 63139. (314) 775-4450.

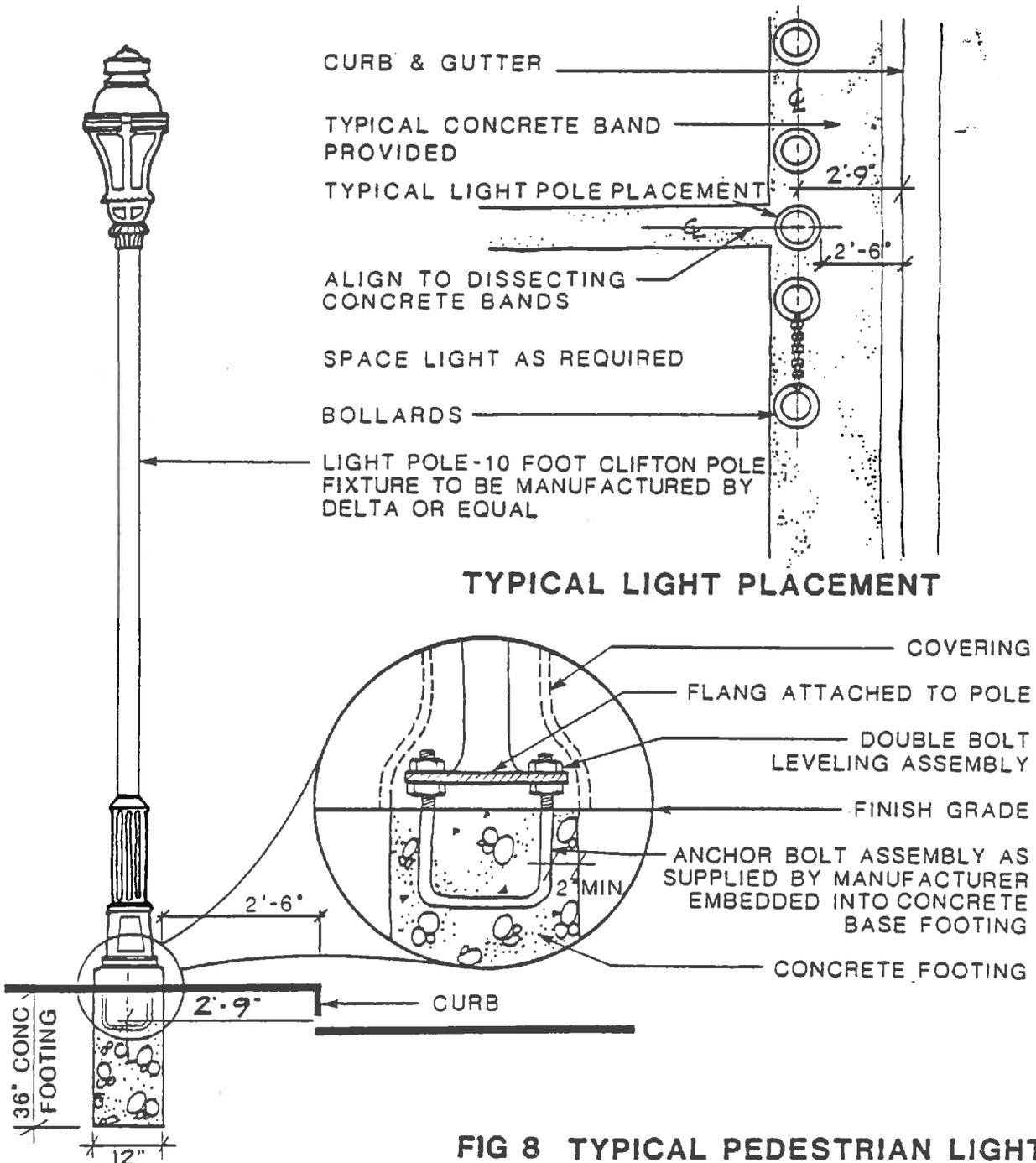
Paint: All work shall conform to the recommendations of the Painting and Decorating Contractors of America for Type 1 work. Galvanized metal shall have 1 coat Palgard Epoxy Coating Metal Primer, 2 coat Palgard Epoxy Coating. Pratt & Lambert has been used herein as a standard of quality. Equivalent products of other manufacturers may be used upon approval.

Manufacturer:

Delta Lighting Systems, Inc. 1328 Virginia Avenue
Baldwin Park, California 91706 (808) 960-7611

Hadco Lighting 100 Craftway
Littlestown, PA.

Welsbach Lighting 240 Sargento,
New Haven, CT.



TYPICAL LIGHT PLACEMENT

FIG 8 TYPICAL PEDESTRIAN LIGHT

Glendale Downtown Development Design Standards Manual

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Trash and Ash Receptacles

Recommended quantity of three trash receptacles and two ash receptacles per block . Receptacles placed at mid-block and at each end. Both receptacle placement should respond to pedestrian use trends of particular block. Careful consideration should be given to avoid vehicular lines of sight at intersections.

Models: Receptacles to be as manufactured by Canterbury International. Trash receptacles to be model #308A or equal, including 166A liner. Ash receptacles to be model #408.

Color: Color to be KHAKI 2124 per Sterling Chemical Coatings Color Guide. 1350 Brannon Avenue, St. Louis, MO 63139. (314) 775-4450.

Paint: All work shall conform to the recommendations of the Painting and Decorating Contractors of America for Type 1 work. Ungalvanized metal shall have 1 coat Palgard Epoxy Coating Metal Primer, 2 coat Palgard Epoxy Coating. Pratt & Lambert has been used herein as a standard of quality. Equivalent products of other manufacturers may be used upon approval.

Manufacturer:

Canterbury International
Post Office Box 5730
Sherman Oaks, California 91413
(213) 936-7111

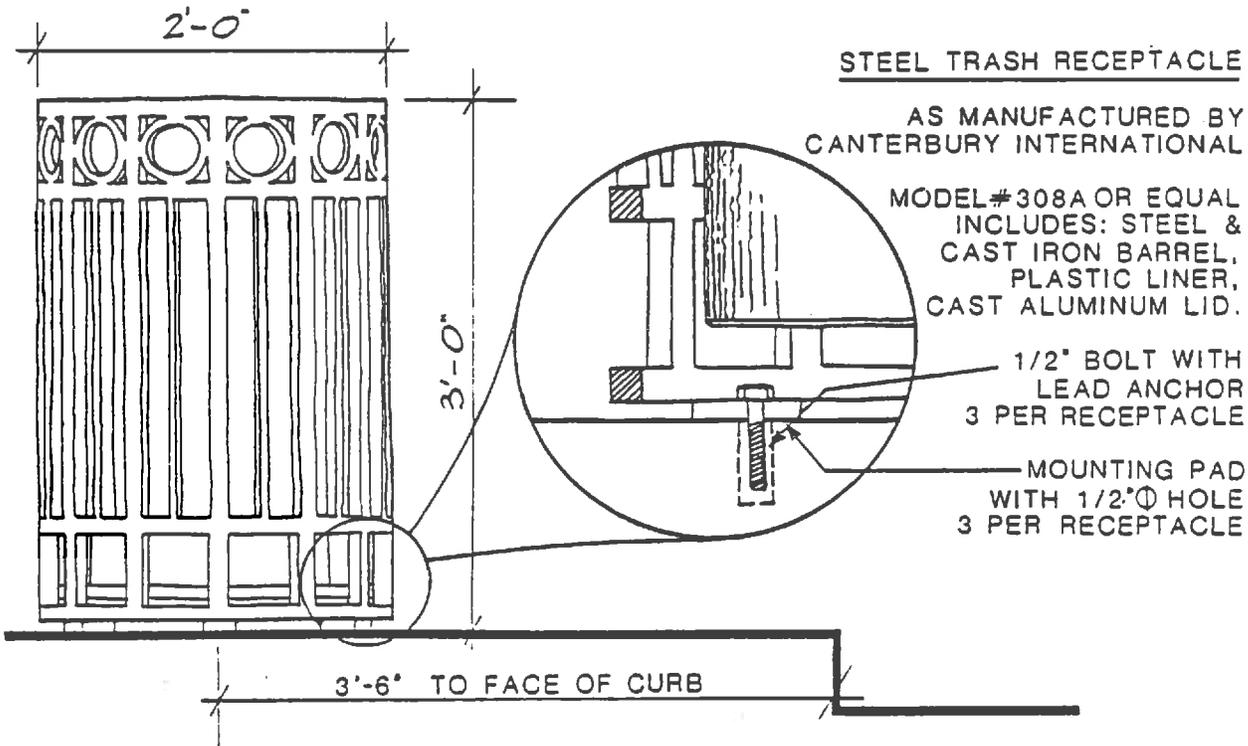
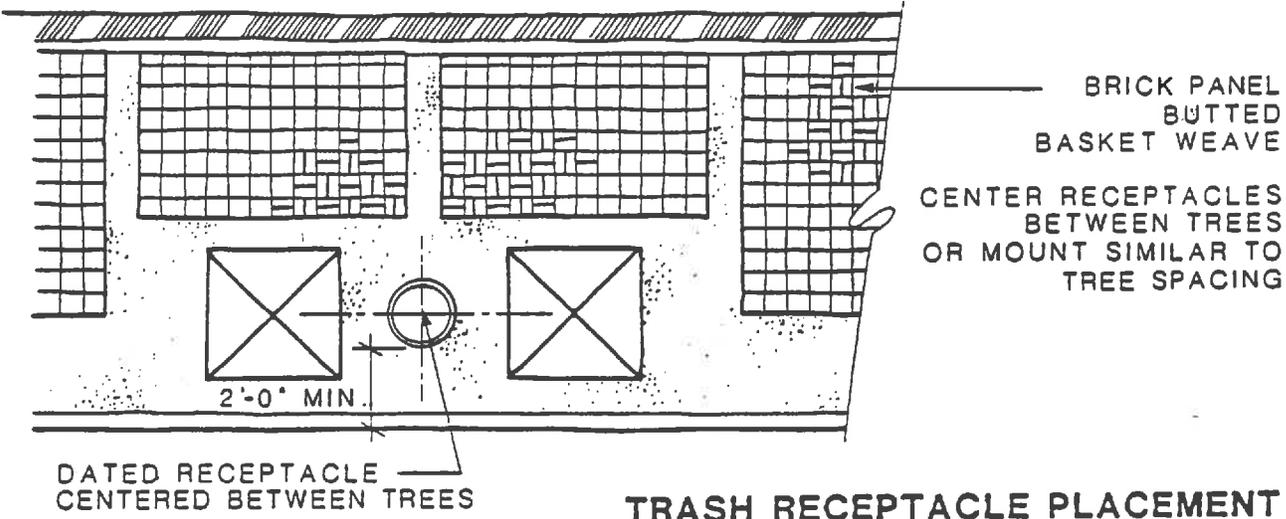
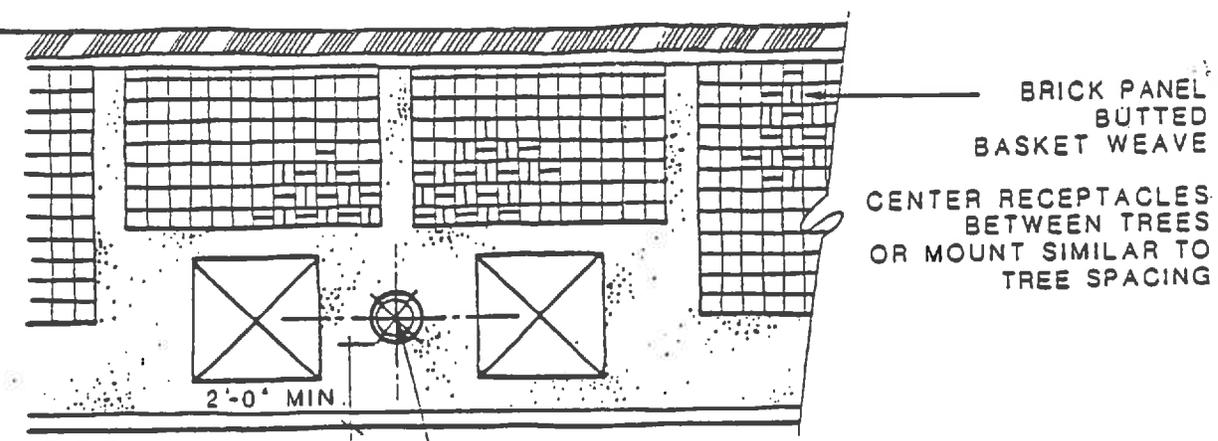


FIG 9 TRASH RECEPTACLE DETAIL

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ASH RECEPTACLE PLACEMENT

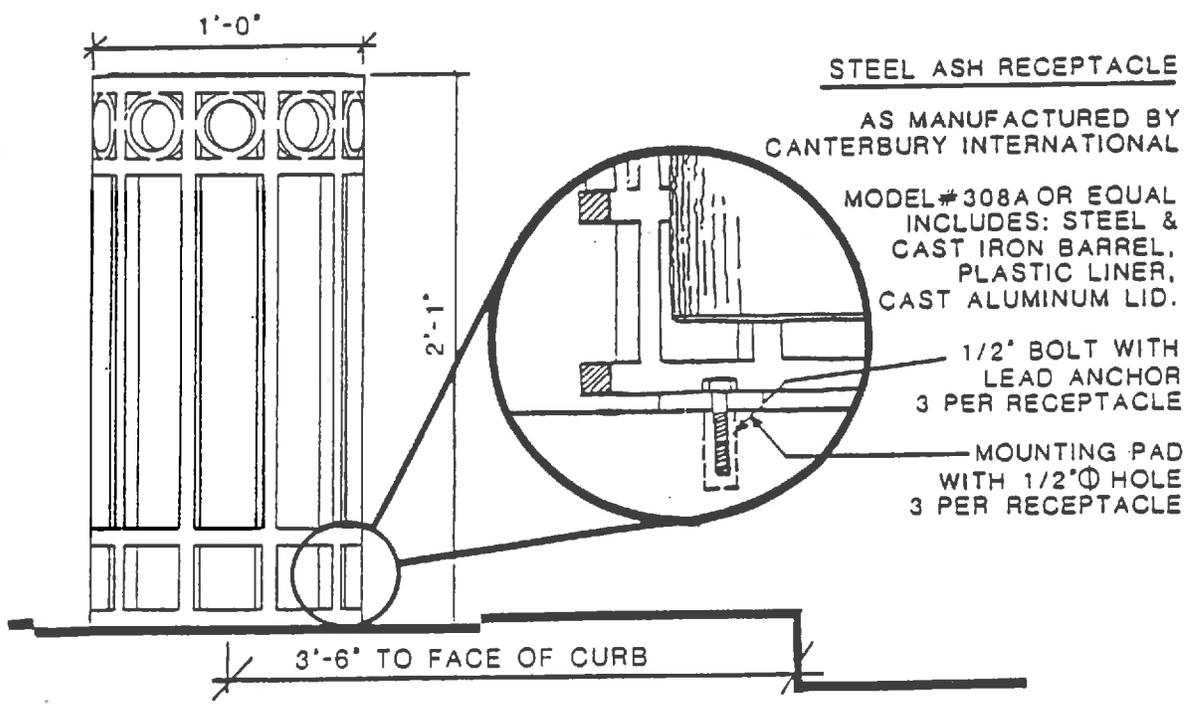


FIG 9A ASH RECEPTACLE DETAIL

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Phone Booth

Phone booths are to be installed as deemed necessary by the City of Glendale, Mountain Bell, and as requested by adjacent land owner. Phone booths are placed between and aligned with street trees.

Models: Phone booth to be as manufactured by King Products Limited, model number 540C with a 54" coin slot height.

Color: Color to be standard black duranodic finish as supplied by manufacturer.

Manufacturer:

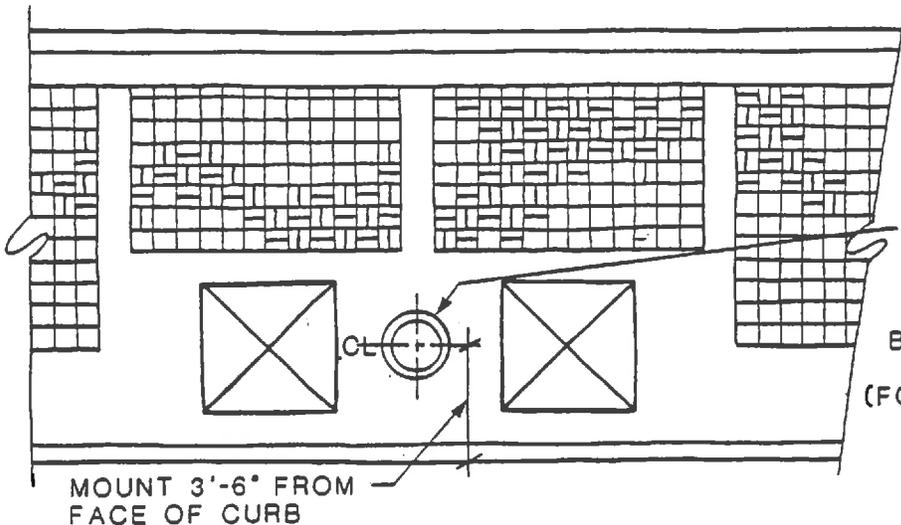
King Products Limited

3150 Wharton Way

Mississauga, Ontario

Canada VX V4X 2C1

Phone: (416) 625-1111

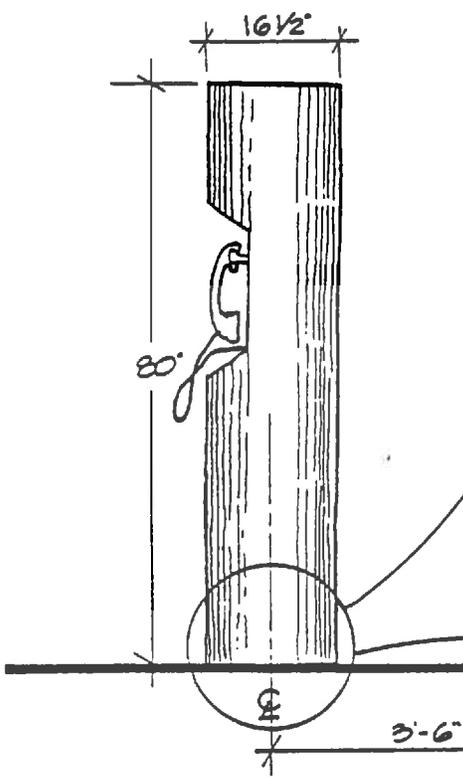


CENTER PHONE BOOTH RECEPTACLE OR PED LIGHT BETWEEN TREES OR BETWEEN BRICK PANELS.

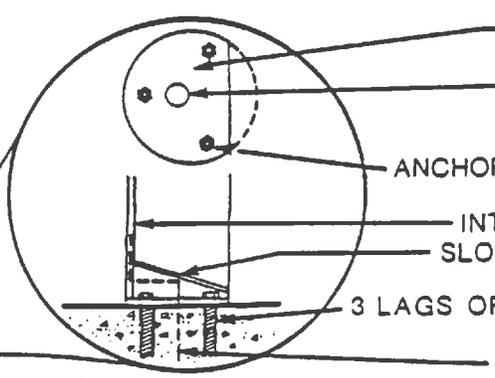
(FOR LEVEL 2 & 3 PHONE PLACEMENT SEE LEVEL 2 & 3 PAVING LAYOUT)

MOUNT 3'-6" FROM FACE OF CURB

TYPICAL PHONE BOOTH PLACEMENT



INSTALL KING PRODUCTS LTD
MODEL NO. 540C -
WITH A 54° COIN SLOT HEIGHT



- ALUMINUM KIOSK
- TELEPHONE AND POWER ENTRY
- ANCHOR BOLTS OR LAGS
- INTERNAL RACEWAY
- SLOPED COVERPLATE
- 3 LAGS OR ANCHOR BOLTS
- TELEPHONE AND POWER ENTRY

FIG 10 TYPICAL PHONE BOOTH LAYOUT

Glendale Downtown Development
DESIGN STANDARDS MANUAL

City of Glendale



Food Carts

To provide for pedestrian needs and to stimulate sidewalk use, it is recommended that mobile food vendors be present in the Glendale Downtown area. A variety of cart models are available to provide many hot and cold snacks such as popcorn, nachos, ice cream, hot dogs, pretzels and snowcones. Carts used within the right-of-way must meet health standards of all local, state and federal governments. Variations from standard manufactured carts are allowed at the discretion and approval of the City of Glendale.

Models: Various models available depending on type of food service. Carts to be as approved by the City of Glendale with Carts of Colorado used as manufactured standard.

Color: Color to vary allowing a variety of color.

Manufacturer:

Carts of Colorado, Inc.
666 Upan Street
Denver, Colorado 80204
(303) 595-0008

Local Representative:

W. R. Val Dere Company
3301 East Corona Avenue
Phoenix, Arizona 85040
(602) 243-6271

Bike Racks

Bike racks similar to those located in Murphy Park are to be located in areas of highest demand as deemed necessary and appropriate by the City of Glendale.

Models: Bike racks are designed for custom manufacturing by local contractors. See standard detail.

Color: Color to be flat black U-1083 per Sterling Chemical Coatings Color Guide. 1350 Brannon Avenue, St. Louis, MO 63139. (314) 775-4450.

Paint: All work shall conform to the recommendations of the Painting and Decorating Contractors of America for Type 1 work. Exterior galvanized metal shall have: 1 coat Derusto Galv-A-grip and 2 coats Effecto Enamel. Pratt & Lambert has been used herein as a standard of quality. Equivalent products of other manufacturers may be used upon approval.

Manufacturer:

Bike racks are designed for custom manufacturing by local contractors.

News Stands

News stands are to be consolidated to one area per block or in as many areas as is deemed necessary by the City of Glendale. News stands are to be located no less than 25' from the adjacent intersection flowline to avoid screening sight . lines of turning cars. News stands to be located no closer than 2'-6" from face of curb and along areas that do not conflict with pedestrian movement.

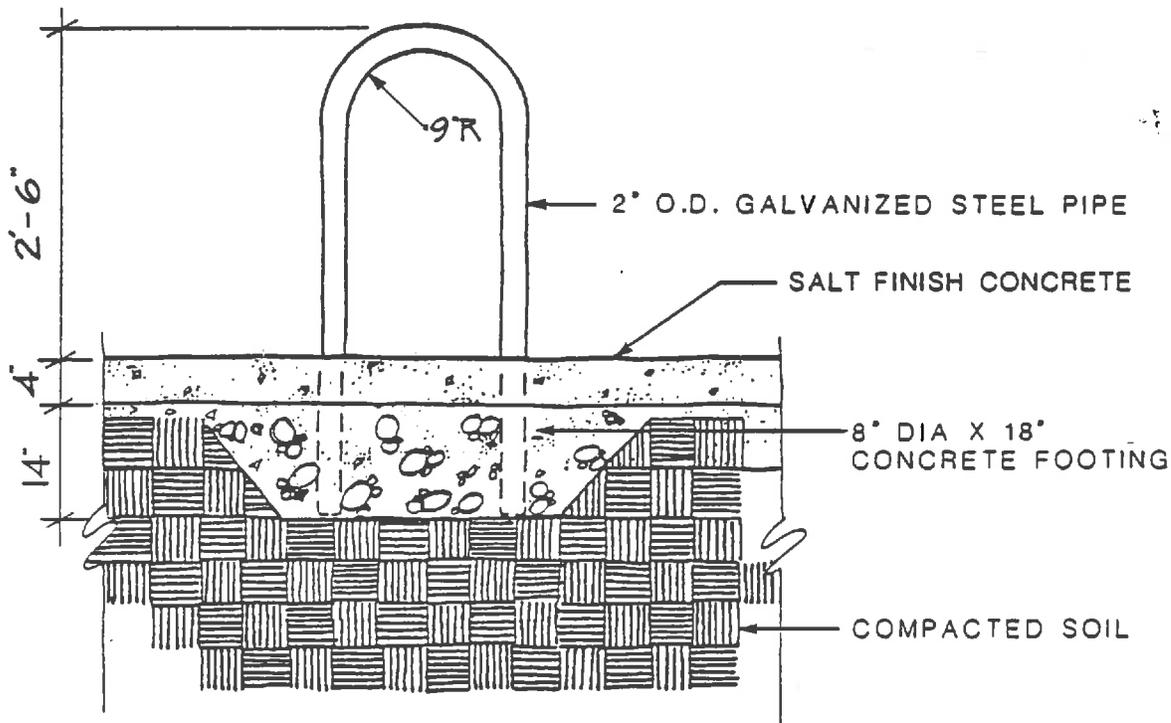
Models: Vending machines to be as manufactured by K-Jack Engineering Model #KJ-50 two feet high by two feet wide or equal as approved by the City of Glendale.

Color: Color to be KHAKI 2124 per Sterling Chemical Coatings Color Guide. 1350 Brannon Avenue, St. Louis, MO 63139. (314) 775-4450.

Paint: All work shall conform to the recommendations of the Painting and Decorating Contractors of America for Type 1 work. Ungalvanized metal shall have: 1 coat of Palgard Epoxy Coating Metal primer and 2 coats of Palgard Epoxy Coating. Galvanized metal shall have: 1 coat Derusto Galv-A-grip and 2 coats Effecto Enamel. Pratt & Lambert has been used herein as a standard of quality. Equivalent products of other manufacturers may be used upon approval.

Manufacturer:

K-Jack Engineering
1522 W. 134th Street
Gardenia, California 90249



SECTION DETAIL

NOTE LOCATION AND PLACEMENT OF BICYCLE RACKS TO BE ADMINISTERED PER DISCRETION OF THE CITY OF GLENDALE.

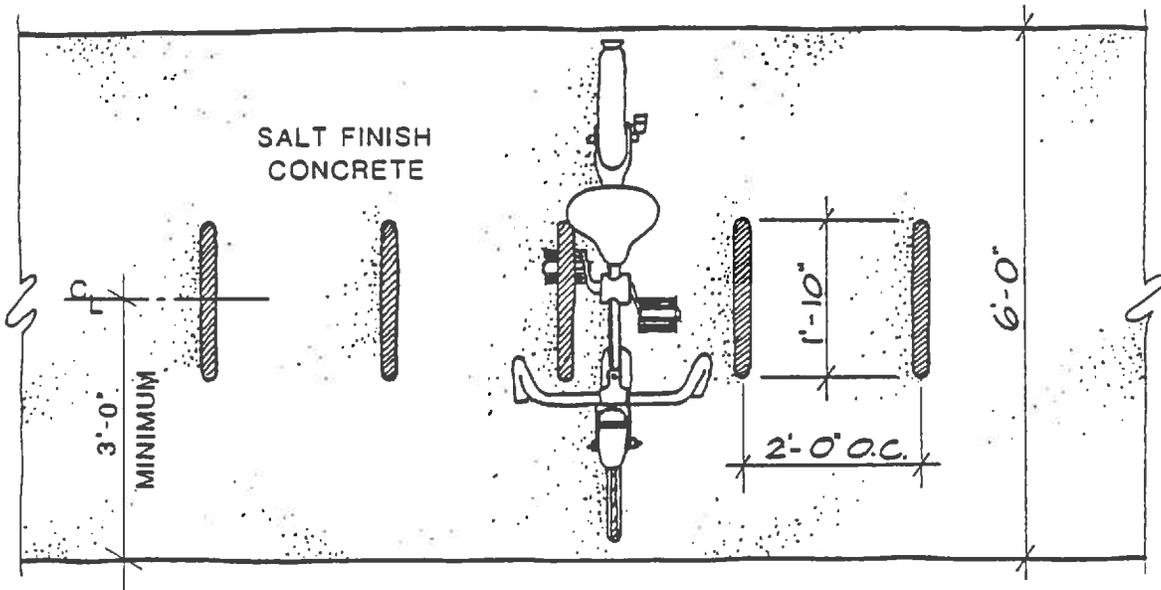
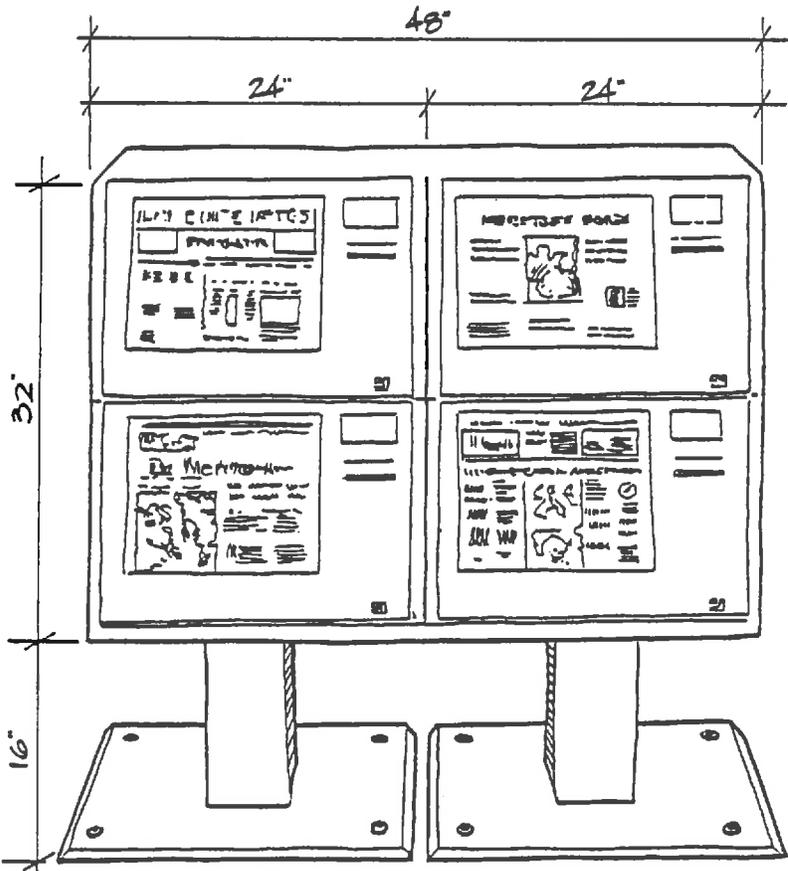
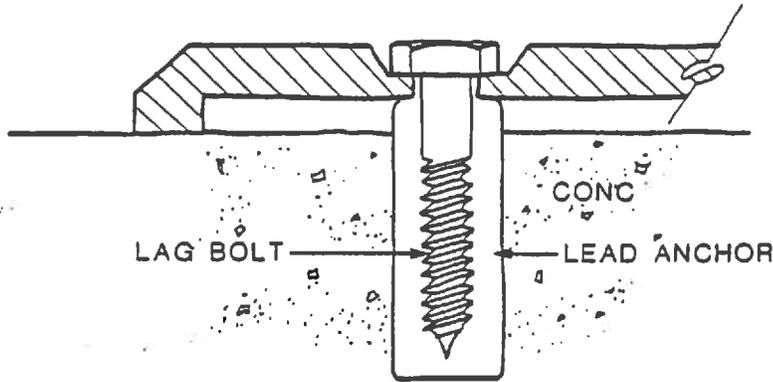


FIG 11 BICYCLE RACK DETAIL

GLENDALE DOWNTOWN DEVELOPMENT
DESIGN STANDARDS MANUAL

City of Glendale





ALL VENDING MACHINES TO BE INSTALLED NO CLOSER THAN 25LF FROM CORNER OF VEHICLE FLOWLINE.

NEWS VENDING MACHINES TO BE CONSOLIDATED & OCCUR IN LIMITED LOCATIONS - AS PER CITY OF GLENDALE APPROVAL. CENTER MACHINES ALONG TREE LINE EQUAL AS PER TRASH RECEPTACLE LOCATIONS.

VENDING MACHINES TO RESEMBLE K. JACK ENG. CO. INC. MODEL KJ-50 2HX2W OR APPROVED BY THE CITY OF GLENDALE.

FASTEN VENDING MACHINE AS PER MANUFACTURERS RECOMMENDATIONS.

FIG 12 NEWS STAND

GLENDALE DOWNTOWN DEVELOPMENT DESIGN STANDARDS MANUAL

City of Glendale



Street and Traffic Lights

Street lights are to be provided along streets in conjunction with pedestrian lights to meet City of Glendale road safety standards and are to be placed at the triangular spacing of 100 feet on center alternating sides of street. Care in placement should be given to include lights as part of the streetscape system. Attention to pavement pattern and alignment of lights with other street furnishings including bollards and pedestrian lights is required.

Models: Light fixtures and poles as provided by the City of Glendale with High Pressure Sodium Bulbs.

Manufacturers:

American Electric

HPS Cutt-off Black

No. 153-H143-AJH

General Electric

DS 400 Black

No. C596N542/C258G150

Holophone

Summerset Black

No. SMST-250HPS-MT-BK-PMSMSTPB

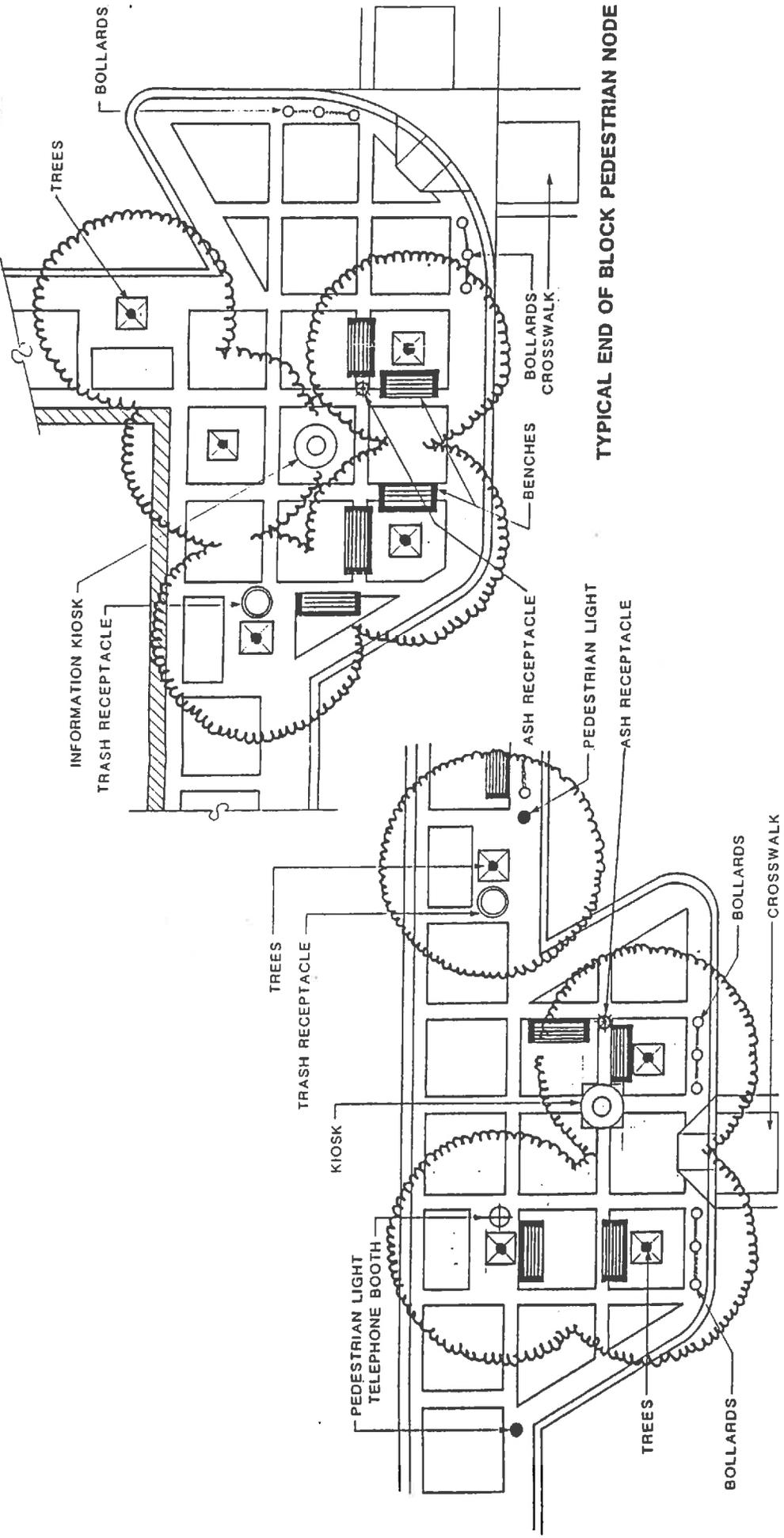


FIG 14 PEDESTRIAN NODES ILLUSTRATIONS

TYPICAL MID-BLOCK PEDESTRIAN NODE

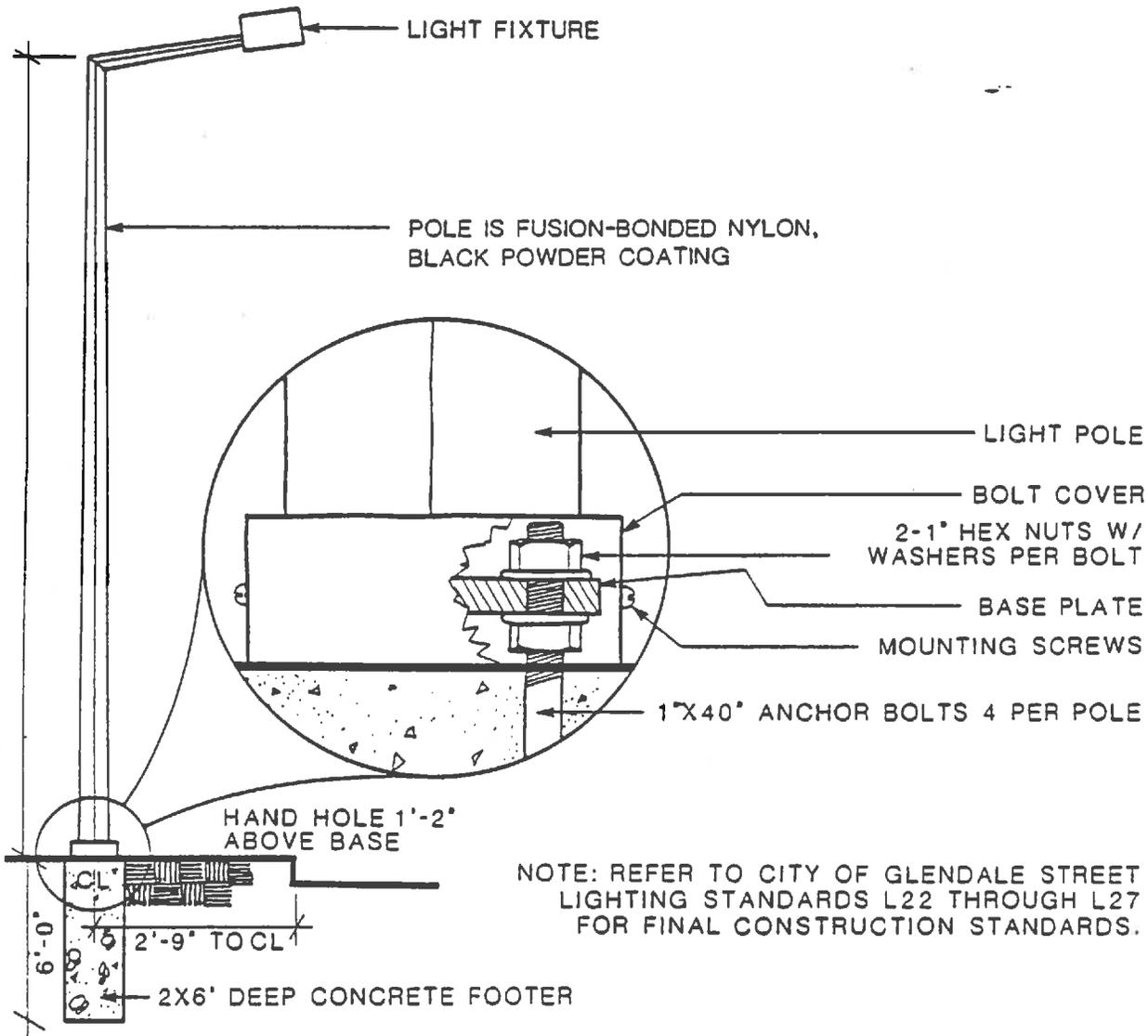


FIG 13 TYPICAL STREET LIGHT STANDARD

GLENDALE DOWNTOWN DEVELOPMENT DESIGN STANDARDS MANUAL

City of Glendale



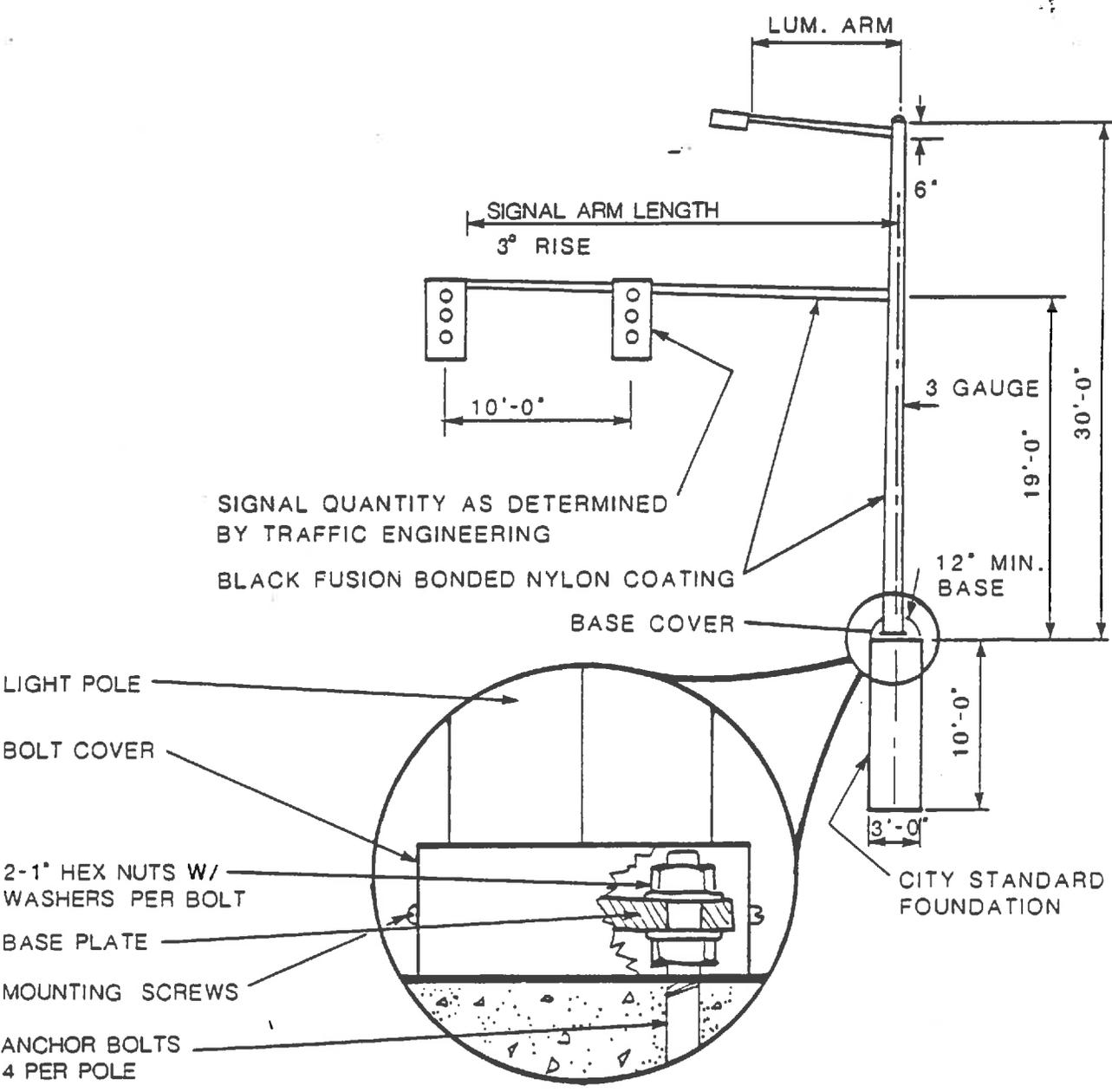


FIG 13A TRAFFIC SIGNAL STANDARD

Pedestrian Nodes

Pedestrian nodes are located at block ends and at selected mid blocks with parallel or diagonal parking. An emphasis is placed on the development of people spaces composed of bench groups facing each other or at ninety degrees to each other for pedestrian seating nodes. Bollards are placed between the benches and the curbs for added safety, tree masses are associated with bench groups for dense shade and locations for phones, information kiosks, and sculptures are provided. Placement of street furnishings responds to the modularity of the pavement pattern and avoid crossing over concrete header lines whenever possible.

Models: Information Kiosks as provided by Poblocki and sons, or as selected by the City Of Glendale.

Color: Color may vary allowing a variety of color

Manufacturer:

Poblocki and Sons, Inc.
PO Box 04665
620 South First Street
Milwaukee, WI 53204

Bus Stops

The bus stops as illustrated, include far side intersection and mid block pullouts designed for articulated buses and typical sidewalk bench bus stop designed for low volume stops. The bus pull out diagrams illustrate the use of amenities including a conceptual bus shelter with the standard teak benches, a standard trash receptacle, bus stop sign, news stand vending machines and trees.

Placement of furnishings consider the typical eight foot brick pavement pattern. Bus shelters anchor major bus stops and should be placed upstream from actual bus stop location for the most efficient access to buses.

Manufactured bus shelters effective for southwestern climates are not available. Design criteria for a sufficient bus shelter should include providing shade to the patrons during the peak use hours of the mornings and afternoons. Vertical shade panels working with a water proof canopy is the minimum required element of an appropriate bus shelter for desert climates. Streetscape design scheme should be considered as a primary factor for final form setting and materials for the bus shelter.

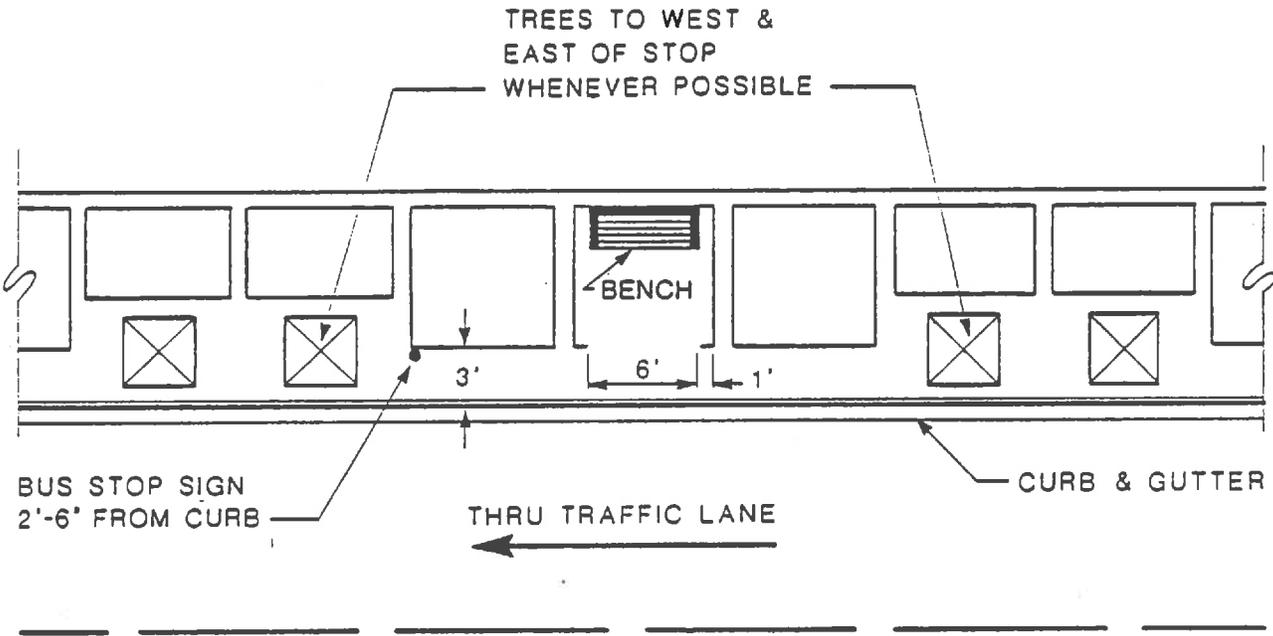


FIG 15 MID-BLOCK BUS STOP

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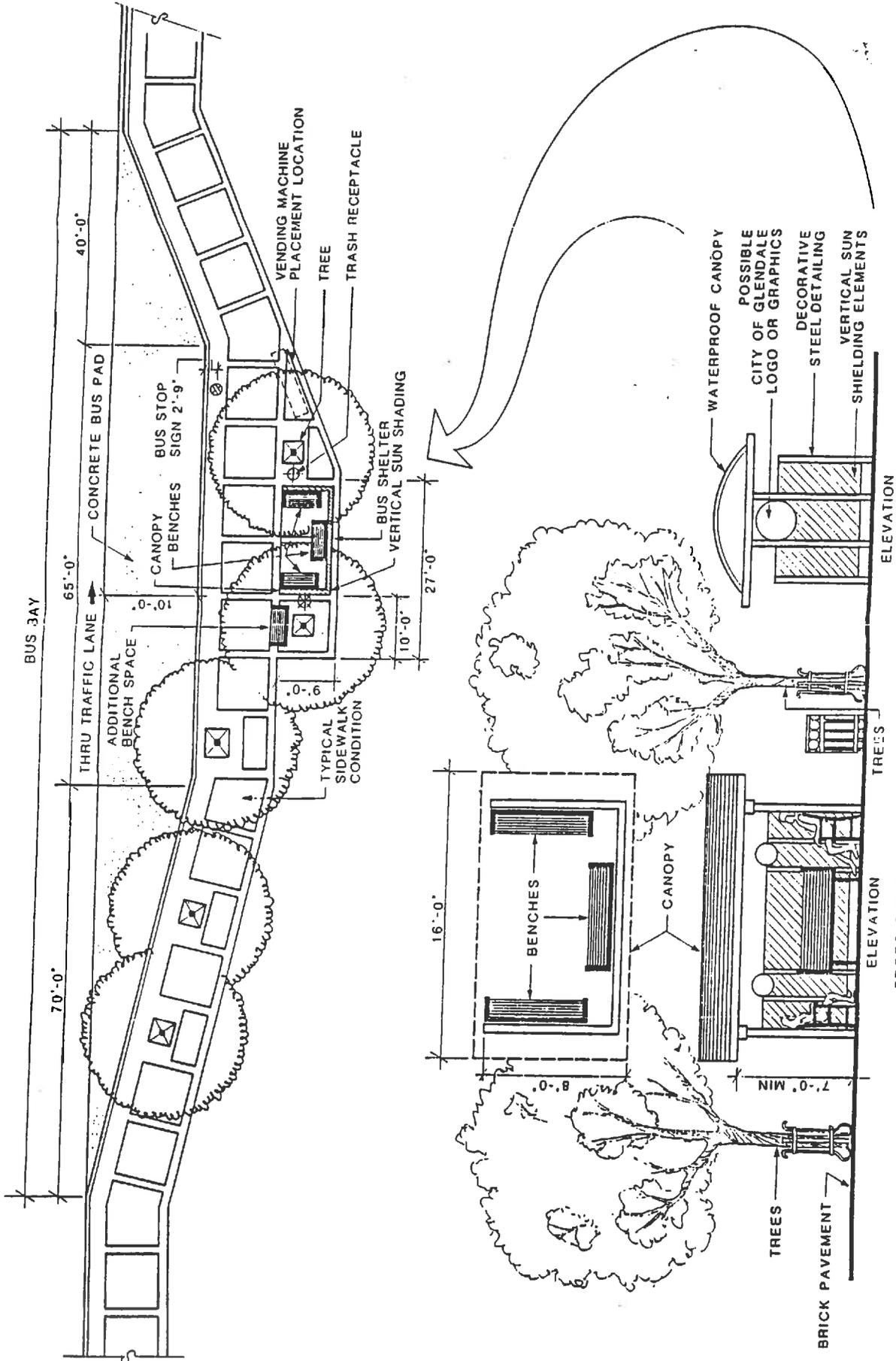


FIG 16 MID-BLOCK BUS BAY LAYOUT.

